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Anthony Lee Washington III
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HDR ENGINEERING, INC.
120 BRENTWOOD COMMONS WAY
SUITE 525
BRENTWOOD, TN 37027
ANTHONY L. WASHINGTON III, PE NO. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B
GENERAL NOTES.....	2C
SPECIAL NOTES.....	2D
ENVIRONMENTAL NOTES.....	2E, 2E1
TABULATED QUANTITIES	2F
UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	4

YEAR	PROJECT NO.	SHEET NO.
2024	STP/HSIP-152(25)	ROADWAY-SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

TENN.	YEAR	SHEET NO.
	2024	1
FED. AID PROJ. NO.	STP/HSIP-152(25)	
STATE PROJ. NO.	17S152-F3-002	
	17S152-F8-002	
	17S152-M3-003	

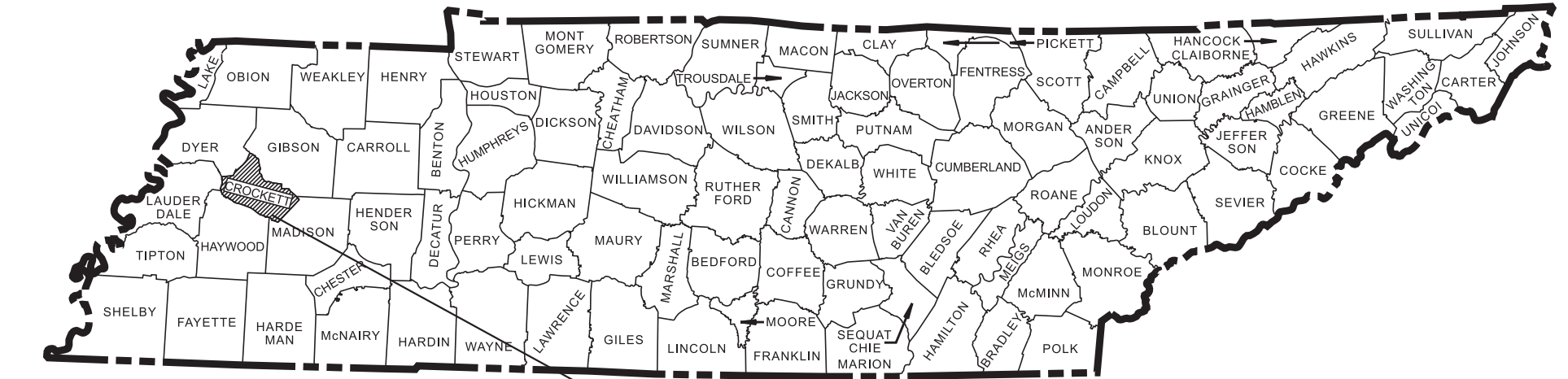
PROJECT TO BE LET WITH:
CROCKETT/GIBSON CO. SR-152
PIN 132542.00

CROCKETT COUNTY

S.R. 152
FROM: L.M.0.00 (S.R. 20)
TO: L.M. 10.76 (GIBSON COUNTY LINE)

RESURFACE & SAFETY
SCRUB SEAL w/ 411D OVERLAY, GUARDRAIL AND PAVEMENT MARKINGS

STATE HIGHWAY NO. 152 F.A.H.S. NO. N/A



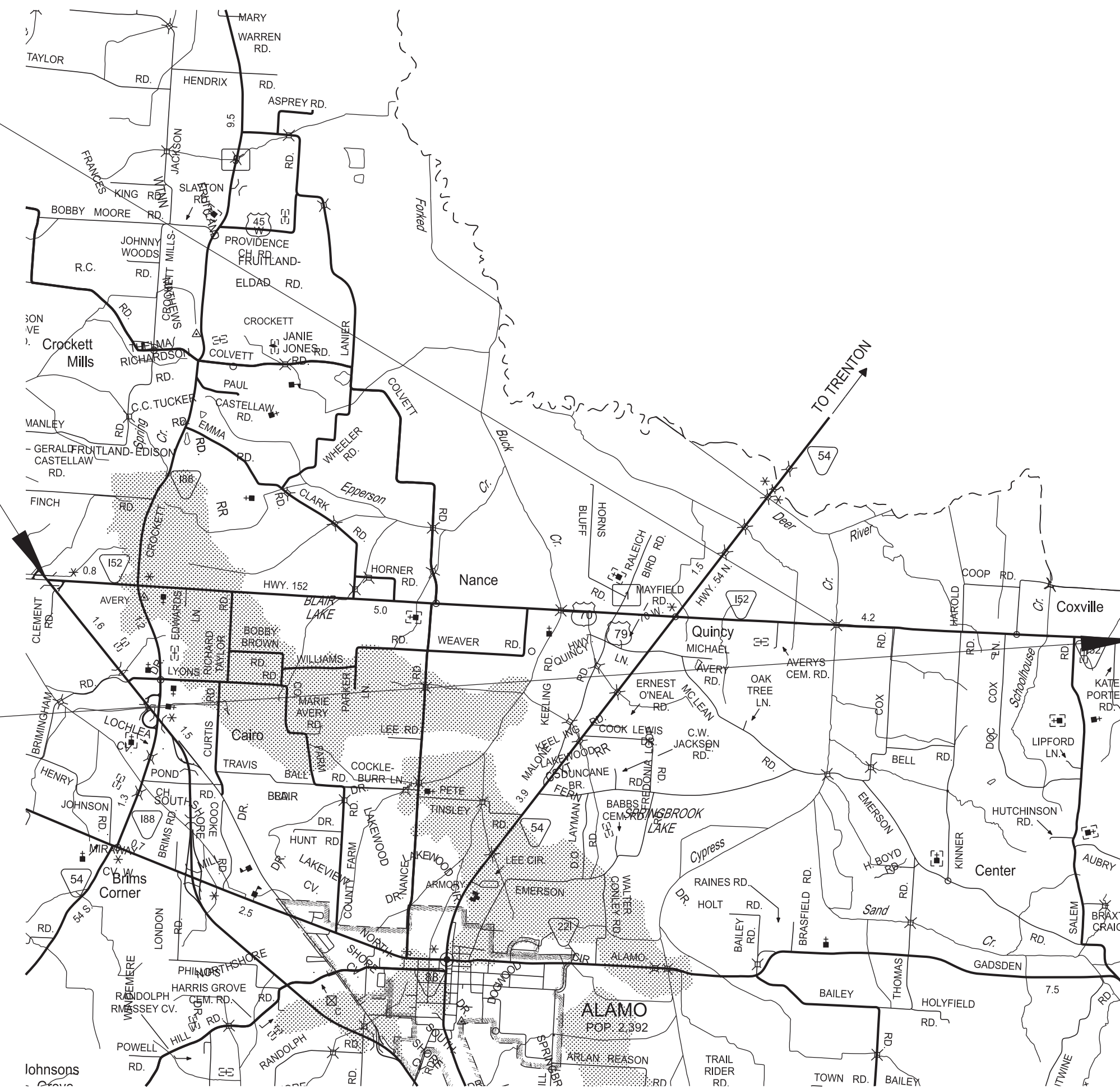
PROJECT LOCATION

BRIDGE ID. # 17SR1520001 17SR1520009 17SR1520003
17SR1520013 17SR1520011 17SR1520005

BRIDGE-DECK-REPAIR PROJECT NO. 17S152-M3-003
SR-152 @ L.M. 7.32

17S152-F3-002
17S152-F8-002
BEGIN PROJECT NO. STP/HSIP-152(25) RESURFACE & SAFETY
L.M. 0.00 (SR-20)

17S152-F3-002
17S152-F8-002
END PROJECT NO. STP/HSIP-152(25) RESURFACE & SAFETY
L.M. 10.76 (GIBSON COUNTY LINE)

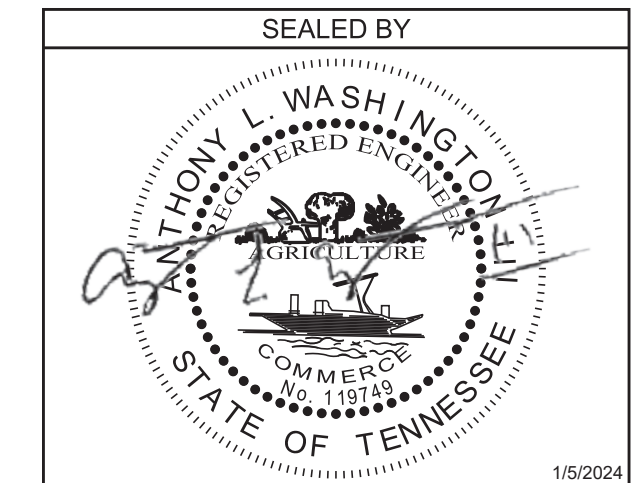


SCALE: 1"= 5280'



PROJECT LENGTH 10.76 MILES
TOTAL LANE MILES RESURFACED 21.52 MILES

NO EXCLUSIONS



APPROVED: *Will Reid*
WILL REID, CHIEF ENGINEER

DATE:

APPROVED: *Howard H. Eley*
HOWARD H. ELEY, COMMISSIONER

TRAFFIC COUNTER AND WEATHER STATIONS

STATION	LOG MILE
TCS018	0.359
TCS016	4.632
TCS015	6.311

TRAFFIC DATA

ADT (2024)	1,791
POSTED SPEED	55 MPH

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT ROAD SP. SV. 2 : RYAN PHILPOTT, P.E.

DESIGNED BY : HDR ENGINEERING, INC.

DESIGNER : ANTHONY L. WASHINGTON III, P.E. CHECKED BY DAVID HORNE, P.E.

P.E. NO. 98043-4283-04 (DESIGN)

PIN NO. 132541.00

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

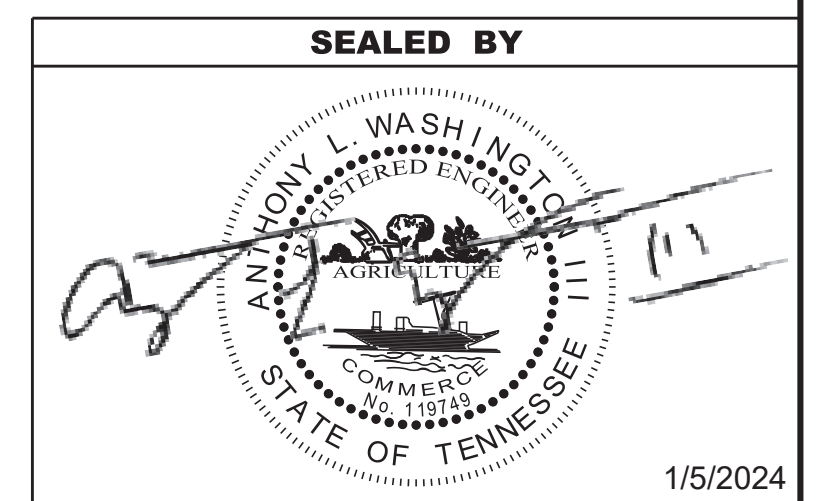
APPROVED: _____
DIVISION ADMINISTRATOR DATE

ROADWAY INDEX

STANDARD ROADWAY DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	1A

SHEET NAME	SHEET NO.	DWG.	REV.	DESCRIPTION
SIGNATURE SHEET	ROADWAY-SIGN1	STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
TITLE SHEET	1			
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A	RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
PROJECT COMMITMENTS	1B	RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
ESTIMATED ROADWAY QUANTITIES	2	RD-L-1	02-20-20	STANDARD LEGEND
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B	RD-L-1A		STANDARD LEGEND
GENERAL NOTES	2C	SAFETY DESIGN AND GUARDRAILS		
SPECIAL NOTES	2D	S-GRS-4	05-04-22	SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL
ENVIRONMENTAL NOTES	2E, 2E1	S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
TABULATED QUANTITIES	2F	S-GRT-2R	06-28-19	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL (RETROFIT)
UTILITY NOTES AND UTILITY OWNERS	3	DESIGN - TRAFFIC CONTROL		
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	4	T-M-1	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
PAVEMENT MARKING DETAILS	5	T-M-3	07-07-23	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
BRIDGE PLANS	B-1	T-M-4	07-17-20	STANDARD INTERSECTION PAVEMENT MARKINGS
		T-M-9A		PAVEMENT MARKINGS AND SIGNING DETAILS FOR RAMP INTERSECTIONS.
		T-M-9B		PAVEMENT MARKINGS AND SIGNING DETAILS FOR RAMP INTERSECTIONS.
		T-M-16	03-04-21	RUMBLE STRIPE INSTALLATION LAYOUT
		T-M-16A	02-03-20	RUMBLE STRIPE DETAILS FOR EDGE OF PAVEMENT AND CENTERLINE
		T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
NOTE: THE ALPHABETICAL LETTERS "I", "O" & "Q" ARE NOT USED IN THE NUMBERING OF SHEETS.				
NO UTILITY SHEETS INCLUDED IN THIS SET OF PLANS				

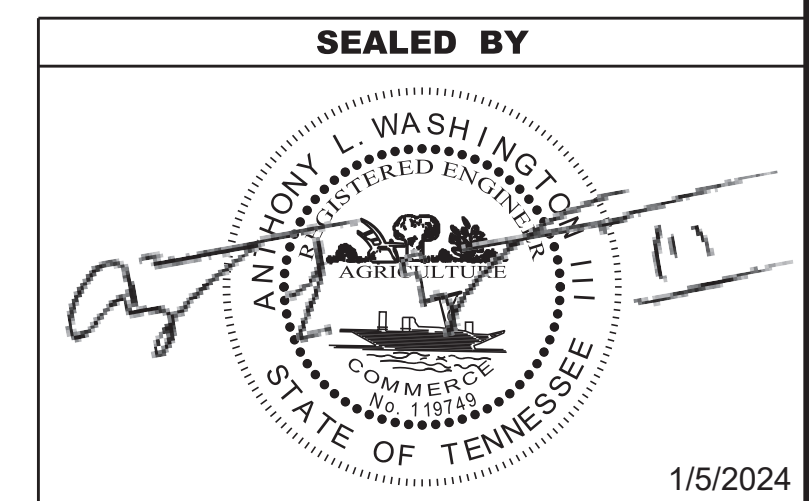


**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

ROADWAY INDEX
AND
STANDARD
ROADWAY
DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL	An Asbestos Containing Material (ACM) survey was completed on Bridge No. 17SR1520003 SR-152 over Cypress Creek LM 7.32 (17-SR152-07.32). No asbestos was detected. Please see the report for further details and photographs. No special accommodations for demolition and waste disposal are anticipated for these structures and the material can be deposited in a C&D landfill. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08.D and 202.03).	Bridge No. 17SR1520003 SR-152 over Cypress Creek LM 7.32 (17-SR152-07.32)



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

PROJECT
COMMITMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	2

ESTIMATED ROADWAY QUANTITIES

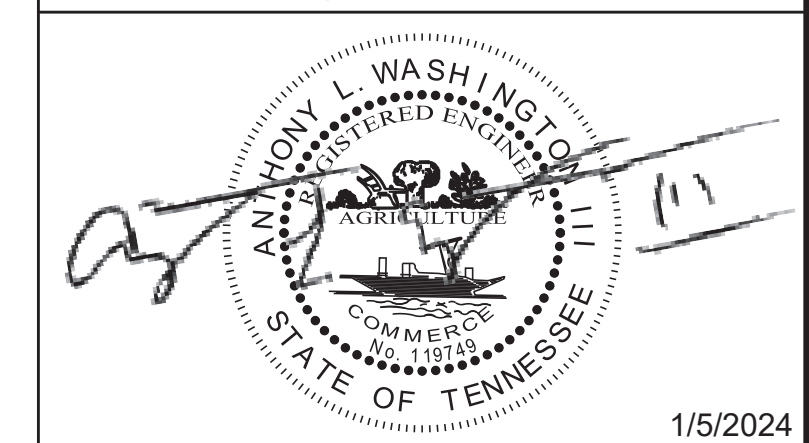
ITEM NO.	DESCRIPTION	UNIT	QUANTITY		TOTAL QUANTITY	
			17S152-F8-002	17S152-F3-002		
	202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	800		800
	203-06	WATER	M.G.	42.7		42.7
(1)	208-01.05	BROOMING & DEGRASSING SHOULDERS	L.M.	21.6		21.6
(2)	303-02	MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)	TON	5725		5725
(3)	307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	276		276
(3)	307-01.08	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	91		91
(4) (5)	403-02.01	TRACKLESS TACK COAT	TON	51		51
(6)	411-01.10	ACS MIX(PG64-22) GRADING D	TON	11094		11094
(7)(8)	411-01.21	LONGITUDINAL JOINT SEALANT	L.M.	11.3		11.3
	411-12.04	SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (4IN WIDTH)	L.M.		17.7	17.7
	414-04.03	ASPHALT EMULSION (SCRUB SEAL)	TON	205		205
	414-04.04	MINERAL AGGREGATE (SCRUB SEAL)	TON	1670		1670
(9)	415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	6400		6400
(10)	705-02.10	GUARDRAIL TRANSITION 27IN TO 31IN	EACH		26	26
(10)	705-04.09	EARTH PAD FOR TYPE 38 GR END TREATMENT	EACH		26	26
(10)	705-06.20	TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH		26	26
(10)	706-01	GUARDRAIL REMOVED	L.F.		1300	1300
(11)	712-01	TRAFFIC CONTROL	LS	0.76		0.76
(12)	712-06	SIGNS (CONSTRUCTION)	S.F.	2025		2025
	716-01.21	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH		775	775
	716-01.22	SNOWPLOWABLE RAISED PAVEMENT MARKERS (MONO-DIR) (1 COLOR)	EACH		4	4
	716-01.23	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (2 COLOR)	EACH		25	25
(13)(14)	716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	70	32	102
(13)(14)(15)	716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	144	170	314
(13)(14)	716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	10		10
(13)(16)	716-02.08	PLASTIC PAVEMENT MARKING (8" DOTTED LINE)	L.F.	26		26
(13)	716-02.12	PLASTIC PAVEMENT MARKING (8IN LINE)	L.M.	0.1		0.1
(13)(14)	716-03.03	PLASTIC WORD PAVEMENT MARKING (STOP AHEAD)	EACH	4		4
(13)(14)	716-04.04	PLASTIC PAVEMENT MARKING (TRANSVERSE SHOULDER)	L.F.		26	26
(13)	716-04.06	PLASTIC PAVEMENT MARKING (WRONG WAY ARROW)	EACH		1	1
(13)	716-04.12	PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F.	100		100
(17)	716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	37.5		37.5
	716-08.09	REMOVAL OF PAVEMENT MARKING (DOTTED LINE)	L.F.	75		75
	716-08.20	REMOVAL OF PAVEMENT MARKING (LINE)	L.M.	37.5		37.5
(13)	716-12.02	ENHANCED FLATLINE THERMO PVM T MRKNG (6IN LINE)	L.M.		37.5	37.5
	717-01	MOBILIZATION	LS	0.76		0.76

FOOTNOTES

- (1) TO BE USED AS DIRECTED BY THE ENGINEER.
- (2) TO BE USED AS DIRECTED BY THE ENGINEER FOR BREAKOUTS. INCLUDES 2165 TONS FOR BREAKOUTS.
- (3) FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM NUMBER 202-03.01.
- (4) INCLUDES 6 TONS FOR SIDE ROADS, PRIVATE DRIVES, FIELD ENTRANCES, BUSINESS ENTRANCES, AND EXTRA WIDTH PAVING.
- (5) THE FINAL SCRUB SURFACE SHALL BE SWEEPED IMMEDIATELY BEFORE APPLYING TACK COAT. THIS SWEEPING IS IN ADDITION TO THE SWEEPING REQUIRED UNDER PROVISION 405SS.
- (6) INCLUDES 1290 TONS FOR SIDE ROADS, PRIVATE DRIVES, FIELD ENTRANCES, BUSINESS ENTRANCES, AND EXTRA WIDTH PAVING.
- (7) USE CRAFCO PAVEMENT JOINT ADHESIVE #34524. PAVON JOINT ADHESIVE BY PAVON CORPORATION OR DENSO TAPE BY DENSO.
- (8) TO BE USED FOR SEALING OF ALL SURFACE LAYER CONSTRUCTION JOINTS ALONG THE TRAVEL LANES AND SHOULDERS AS DIRECTED BY THE ENGINEER.
- (9) FOR INTERSECTIONS OF S.R. 20, S.R. 188, S.R. 54, BRIDGES AND BRIDGE/ROADWAY TRANSITIONS.
- (10) SEE PROPOSED GUARDRAIL (RESURFACING), SHEET 2F.
- (11) THIS ITEM INCLUDES TRAFFIC CONTROL FOR ANY BRIDGE REPAIR WORK. ALL SIGNING SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- (12) THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION). SEE TRAFFIC CONTROL TABULATION (RESURFACING), SHEET 2F.
- (13) ITEM TO BE USED AS PERMANENT MARKING ONLY.
- (14) THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- (15) ALL STOP BARS ON SIDE STREETS WILL BE RETRACED AS DIRECTED BY THE ENGINEER.
- (16) TO BE USED FOR TURN LANES.
- (17) ITEM TO BE USED FOR TEMPORARY PAVEMENT MARKING ONLY.

THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT

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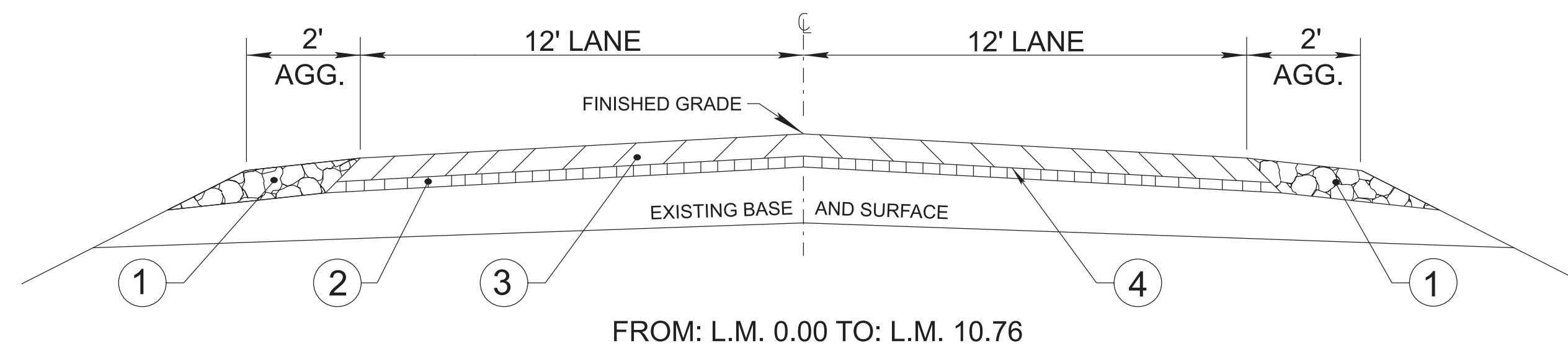


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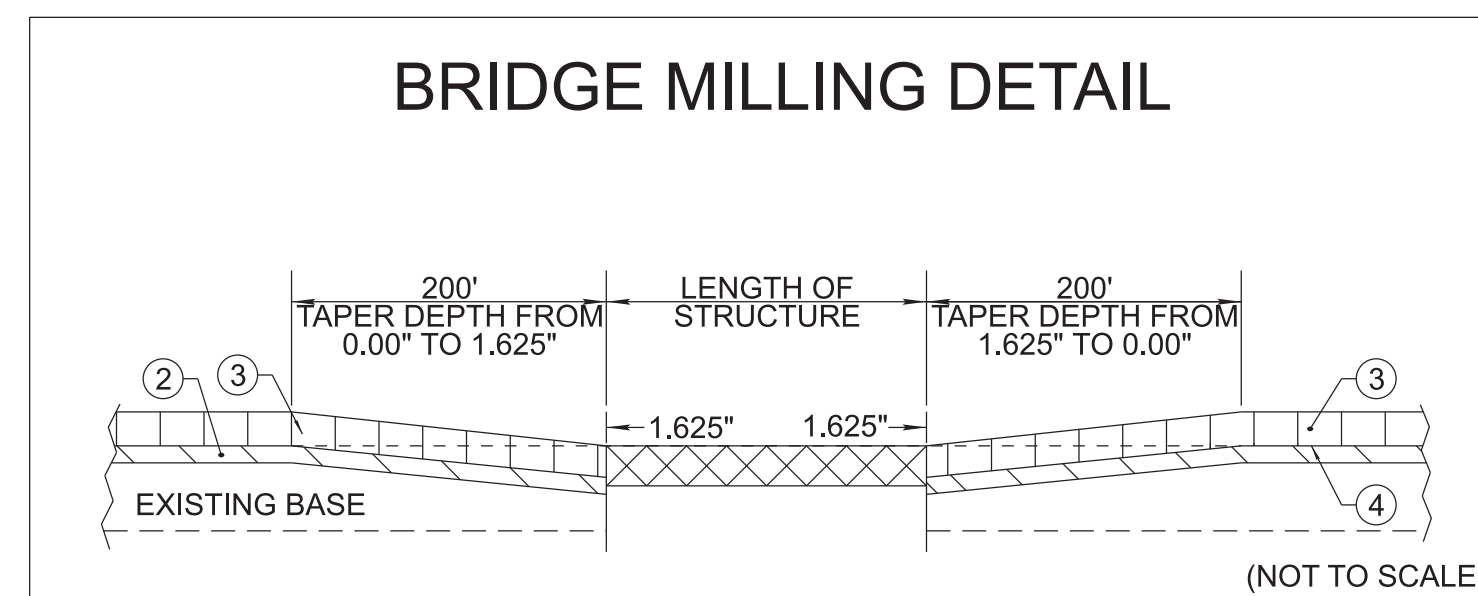
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	2B

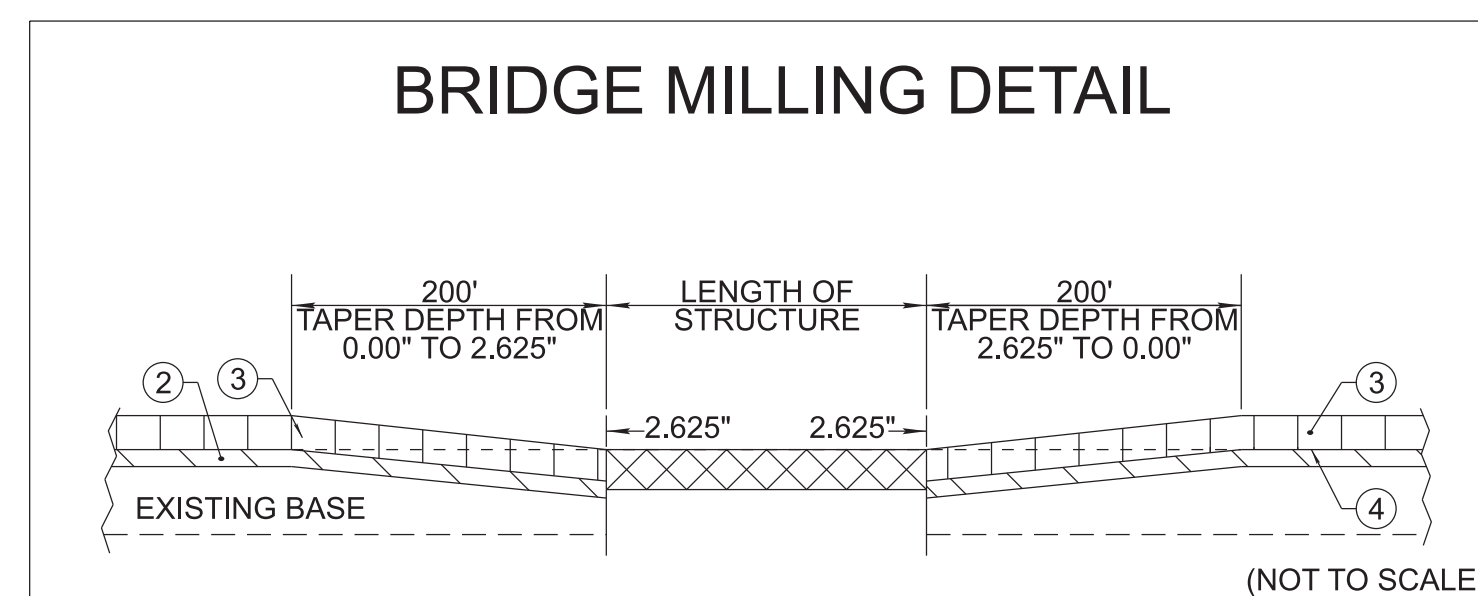


BRIDGE NOTES	
THE CONTRACTOR SHALL: TAPER COLD PLANING FROM 0.00" TO 1.625" DEPTH FROM 200 FT. BEFORE THE STRUCTURE TO THE BEGINNING STRUCTURE AND FROM THE END OF THE STRUCTURE TO 200 FT. AFTER THE FOLLOWING STRUCTURES	
LOCATION	BRIDGE NO.
L.M. 7.18	17SR1520009
L.M. 7.32	17SR1520003
L.M. 9.29	17SR1520011
L.M. 10.31	17SR1520005

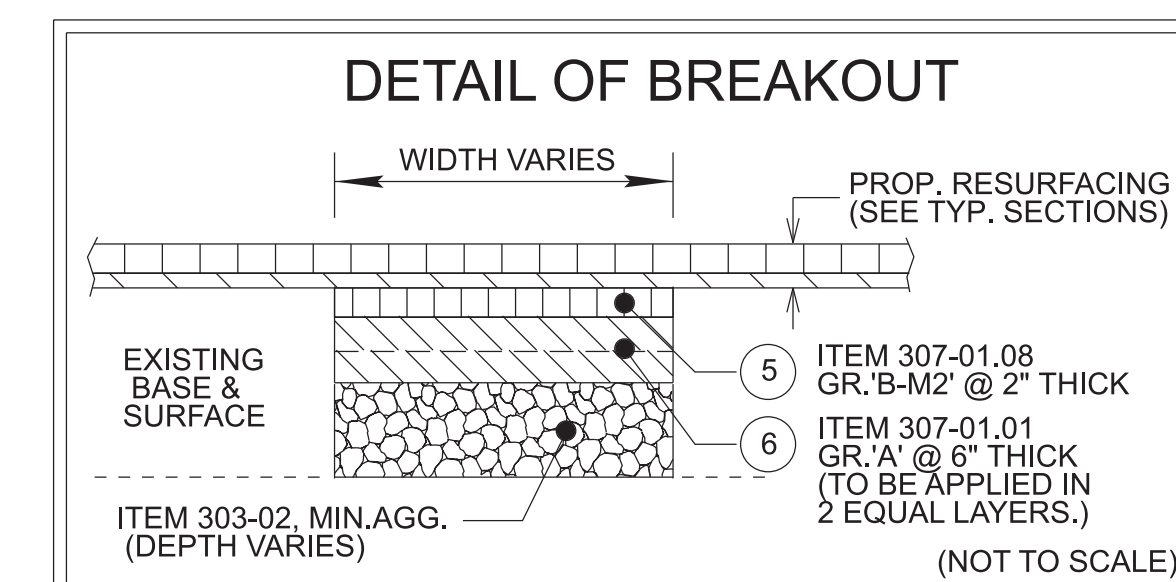


BRIDGE RECOMMENDATIONS			
LOCATION	BRIDGE NO.	LENGTH (FT.)	TREATMENT
L.M. 4.75	17SR1520001	87.00	COLD PLANE 2.625" AND PAVE WITH 1.625" 411D AND SCRUB SEAL
L.M. 7.18	17SR1520009	28.00	LEAVE AS IS
L.M. 7.32	17SR1520003	85.00	SEE BRIDGE REPAIR PLANS
L.M. 7.41	17SR1520013	22.10	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 9.29	17SR1520011	27.00	COLD PLANE 1.625" AND PAVE WITH 1.625" 411D AND SCRUB SEAL
L.M. 10.31	17SR1520005	73.00	COLD PLANE 1.625" AND PAVE WITH 1.625" 411D AND SCRUB SEAL

BRIDGE NOTES	
THE CONTRACTOR SHALL: TAPER COLD PLANING FROM 0.00" TO 2.625" DEPTH FROM 200 FT. BEFORE THE STRUCTURE TO THE BEGINNING STRUCTURE AND FROM THE END OF THE STRUCTURE TO 200 FT. AFTER THE FOLLOWING STRUCTURES	
LOCATION	BRIDGE NO.
L.M. 4.75	17SR1520001



PROPOSED PAVEMENT SCHEDULE	
1 MINERAL AGGREGATE BASE @ 2.00"± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "C OR D"	5 BITUMINOUS BASE COURSE (BINDER) @ 2.00"± THICK (APPROX. 226.00 LBS./S.Y.) ITEM 307-01.08 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "B-M2" (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY)
2 SCRUB SEAL ITEM 414-04.03 ASPHALT EMULSION (SCRUB SEAL) ASPHALT EMULSION (AT 0.25 - 0.35 GAL./S.Y.) ASPHALT EMULSION (AT 0.30 - 0.40 GAL./S.Y.) (TO BE USED FOR COLD PLANING SECTIONS) ITEM 414-04.04 MINERAL AGGREGATE (SCRUB SEAL) MINERAL AGGREGATE (AT 18 - 25 LBS./S.Y.)	6 BITUMINOUS BASE COURSE (BLACK BASE) @ 6.00"± THICK (APPROX. 690.00 LBS./S.Y.) ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "A" (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY) COLD PLANING @ 1.625"± THICK (APPROX. 170.625 LBS./S.Y.) ITEM 415-01.02 COLD PLANING BITUMINOUS PAVEMENT S.Y. (TO BE USED FOR BRIDGES 17SR1520009, 17SR1520003, 17SR1520011, & 17SR1520005) (TO BE USED FOR INTERSECTIONS OF SR-20, SR-188, AND SR-54)
3 ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25"± THICK (APPROX. 132.50 LBS./S.Y.) ITEM 411-01.10 ACS MIX (PG64-22) GRADING "D"	COLD PLANING @ 2.625"± THICK (APPROX. 275.625 LBS./S.Y.) ITEM 415-01.02 COLD PLANING BITUMINOUS PAVEMENT S.Y. (TO BE USED FOR BRIDGE 17SR1520001)
4 TACK COAT (TC) ITEM 403-02.01 TRACKLESS TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD.	



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1/5/2024

NOT TO SCALE

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE**

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

- (1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.
- (3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (5) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
 - a. SHOULDERS SHALL BE BROOMED AND DE-GRASSED AND MATERIAL SHALL BE PICKED UP AND REMOVED. THIS WILL BE PAID FOR UNDER ITEM NO. 208-01.05.
 - b. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PAVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

PAVEMENT

PAVING

- (1) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (1) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (2) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (3) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (4) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

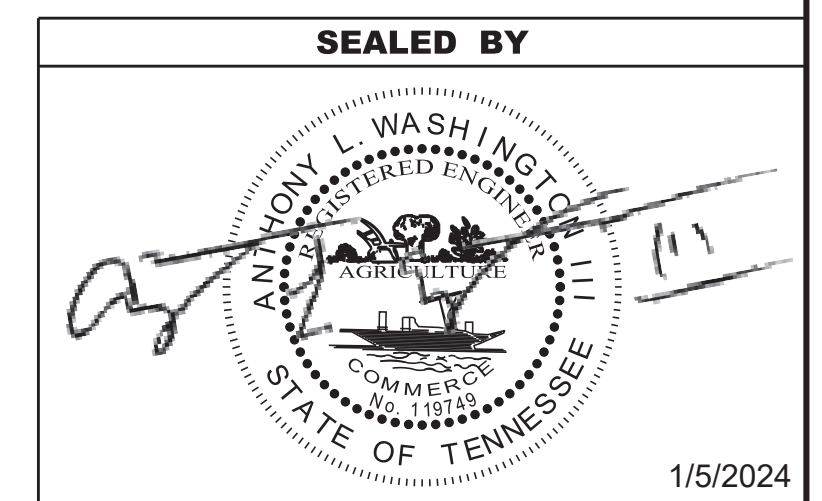
- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	2C



STATE OF TENNESSEE
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GENERAL
NOTES

SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	2D

RESURFACING

- (1) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (2) THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS TO ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.
- (3) FEATHER SURFACE MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.
- (6) THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITH 96 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED, EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.

PAVEMENT MARKING

- (1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.
- (2) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

JOINT SEALANTS

- (1) THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN THE SURFACE LIFT SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE REHEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (2) PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSED AIR OR A POWER SWEEPER.
- (3) TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENT TO PREVENT DAMAGE FROM TRACKING.

SIGNS

- (1) IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS

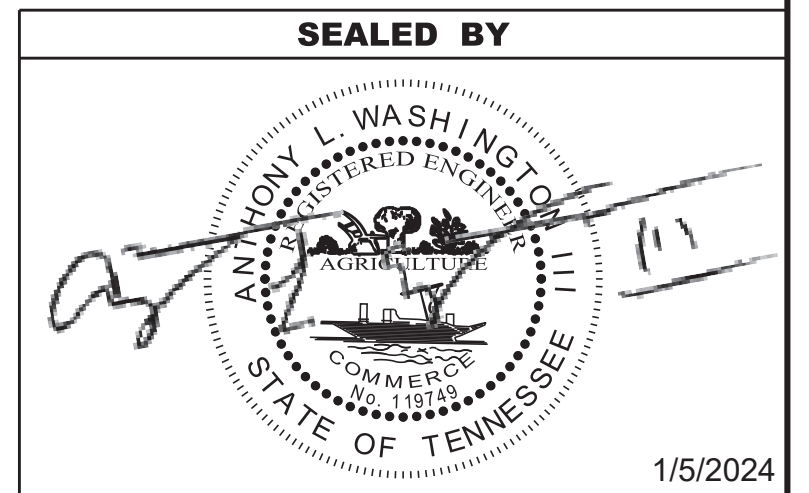
- (1) ITEM 303-02 TO BE PLACED BEFORE SURFACING MATERIAL.
- (2) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION. ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS OR NON-WORK DAYS.
- (2) THE CONTRACTOR SHALL COORDINATE WORK ZONE AND TRAFFIC CONTROL ACTIVITIES WITH THE CONTRACTOR OF CROCKETT-GIBSON COUNTIES PIN 132542.00 (SR-152, FROM CROCKETT COUNTY LINE (GIBSON CO. SEQ. 1) TO SR-366 (SEQ. 3 IN GIBSON CO.)).

SCRUB SEAL

- (1) BEFORE PLACING SCRUB SEAL, ITEM NOS. 414-04-03 AND 414-04.04, THE CONTRACTOR IS REQUIRED TO REMOVE ANY EXISTING THERMOPLASTIC PAVEMENT MARKINGS THAT ARE TO BE COVERED BY SCRUB SEAL, INCLUDING ALL LANE LINES AND SPECIALTY MARKINGS. THE CONTRACTOR SHALL ONLY REMOVE PAVEMENT MARKINGS THAT ARE TO BE COVERED DURING THAT DAY'S CONSTRUCTION ACTIVITIES. THE CONTRACTOR IS TO TAKE EXTRA CARE TO ENSURE THAT THE EXISTING ASPHALT SURFACE IS NOT DAMAGED DURING THERMOPLASTIC REMOVAL. THE ROADWAY MUST BE FREE OF EXCESS DUST OR DEBRIS AS A RESULT OF THERMOPLASTIC REMOVAL BEFORE SCRUB SEAL IS TO BE PLACED.



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

SPECIAL
NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	2E

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES COLD PLANING EXISTING ASPHALT, RESURFACING, PAVEMENT MARKINGS, GUARDRAIL UPGRADES, TEMPORARY TRAFFIC CONTROL, AND BROOMING AND DEGRASSING SHOULDERS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

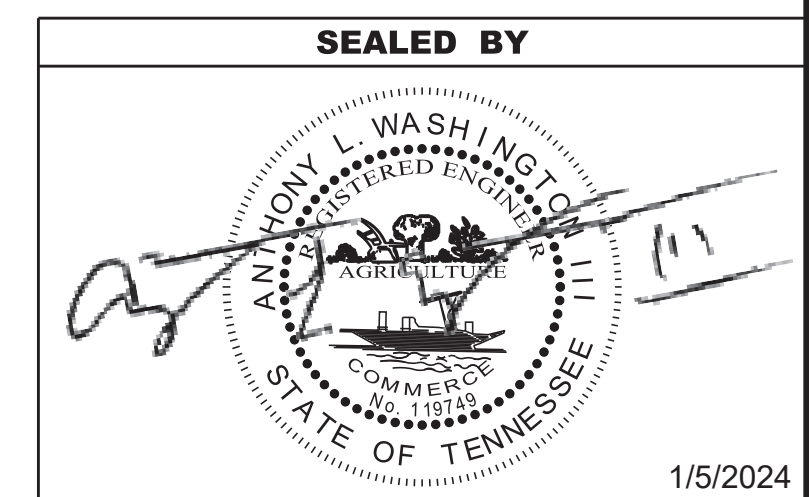
- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.



STATE OF TENNESSEE
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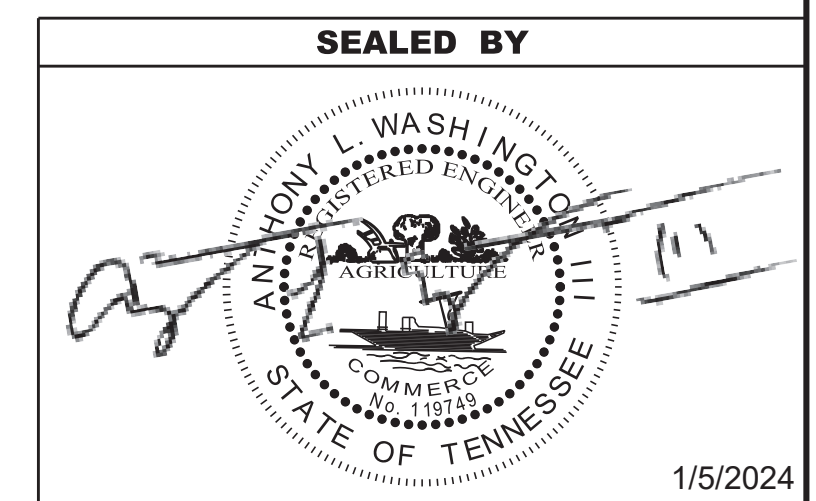
ENVIRONMENTAL
NOTES

ENVIRONMENTAL NOTES (CONT.)

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	2E1

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.



**STATE OF TENNESSEE
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**ENVIRONMENTAL
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	2F

PROPOSED GUARDRAIL (RESURFACING)

SIDE	LOG MILE	GUARDRAIL			TERMINAL ANCHORS	
		GUARDRAIL TRANSITION 27 IN TO 31 IN 705-02.10 (EACH)	EARTH PAD FOR TYPE 38 GR. END TREATMENT 705-04.09 (EACH)	GUARDRAIL REMOVED 706-01 (L.F.)	TYPE 38 MASH TL3 (46.875') 705-06.20 (EACH)	
LT	RT					
	X	0.050	1	1	50	1
X		0.111	1	1	50	1
	X	2.696	1	1	50	1
X		2.770	1	1	50	1
X		2.798	1	1	50	1
	X	2.798	1	1	50	1
X		4.714	1	1	50	1
	X	4.714	1	1	50	1
X		4.839	1	1	50	1
	X	4.944	1	1	50	1
	X	4.950	1	1	50	1
	X	5.026	1	1	50	1
X		7.162	1	1	50	1
	X	7.162	1	1	50	1
	X	7.180	1	1	50	1
X		7.186	1	1	50	1
	X	7.297	1	1	50	1
X		7.300	1	1	50	1
	X	7.341	1	1	50	1
X		7.345	1	1	50	1
X		7.391	1	1	50	1
	X	7.391	1	1	50	1
	X	7.426	1	1	50	1
X		7.429	1	1	50	1
X		Near SR20 EB Ramps	1	1	50	1
X		Near SR20 EB Ramps	1	1	50	1
TOTALS		26	26	1300	26	

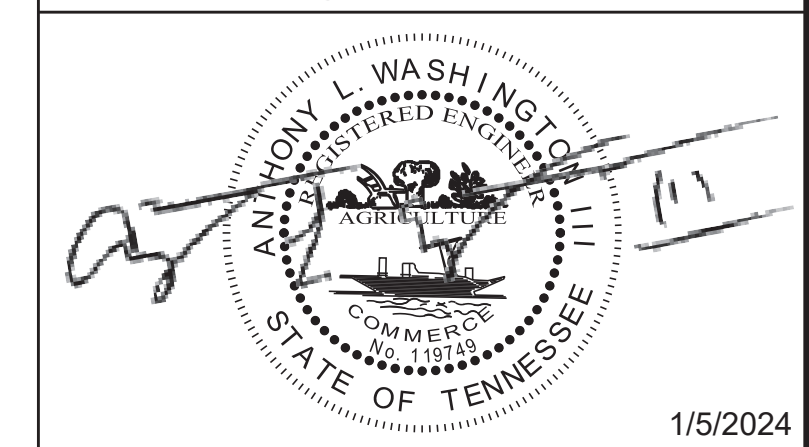
TRAFFIC CONTROL SIGN TABULATION (RESURFACING)

M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES		S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
		L	x W			
G20-1	ROAD WORK NEXT 14 MILES	48"	x 24"	8	1	8
G20-2	END ROAD WORK	48"	x 24"	8	23	184
W8-11	UNEVEN LANES	48"	x 48"	16	58	928
W8-15	GROOVED PAVEMENT	48"	x 48"	16	21	336
W8-15P	MOTORCYCLE PLAQUE	30"	x 24"	5	21	105
W20-1	ROAD WORK 1 MILE	48"	x 48"	16	1	16
W20-1	ROAD WORK 1/2 MILE	48"	x 48"	16	1	16
W20-1	ROAD WORK 1000 FEET	48"	x 48"	16	1	16
W20-1	ROAD WORK AHEAD	48"	x 48"	16	21	336
W20-4	ONE LANE ROAD 1000 FT - PORTABLE	48"	x 48"	16	1	16
W20-7A	FLAGGER SYMBOL - PORTABLE	48"	x 48"	16	1	16
W21-2	FRESH OIL - PORTABLE	48"	x 48"	16	1	16
W21-5	SHOULDER WORK	48"	x 48"	16	2	32

THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.

TOTAL	2025
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SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

UTILITY NOTES

UTILITY

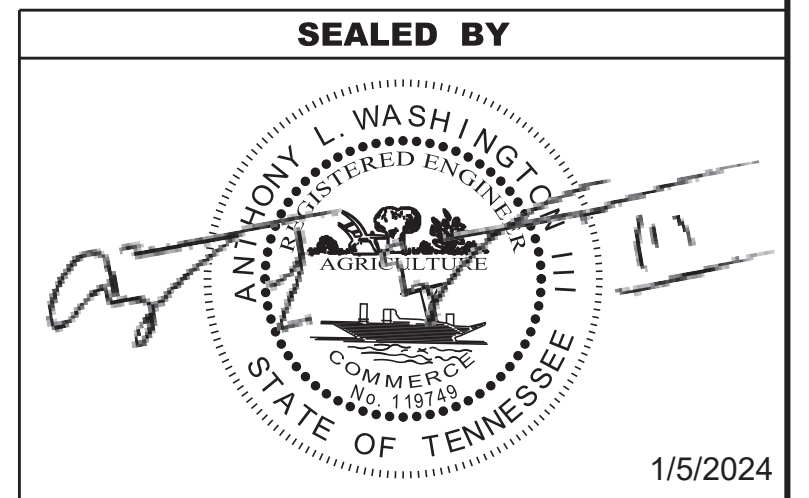
- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

ELECTRIC
GIBSON ELECTRIC MEMBERSHIP CORP.
 1207 S. COLLEGE ST.
 TRENTON, TN 38382
 CONTACT: STACEY NICKS
 OFFICE PHONE: 731 855 4660
 CELL PHONE: 731 446 3236
 Email: SNICKS@GIBSONEMC.COM

TELEPHONE:
AT&T
 315 E. COLLEGE STREET
 JACKSON, TN. 38301
 CONTACT: DANIEL R. POTTS
 OFFICE PHONE: 901 488 2359
 CELL PHONE: ___ ___ ___
 Email: DP7607@ATT.COM

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	3



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES
 AND
 UTILITY OWNERS

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	4

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
 - a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
 - b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

- a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.
- d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

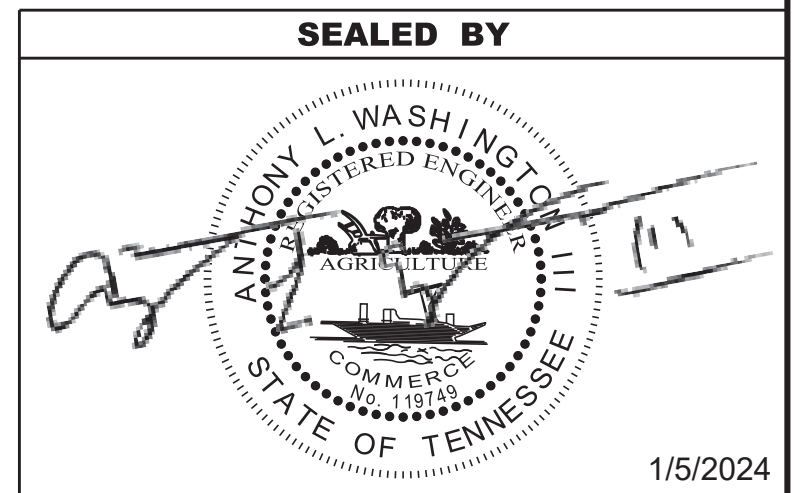
1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.
 - a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
 - b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

- SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

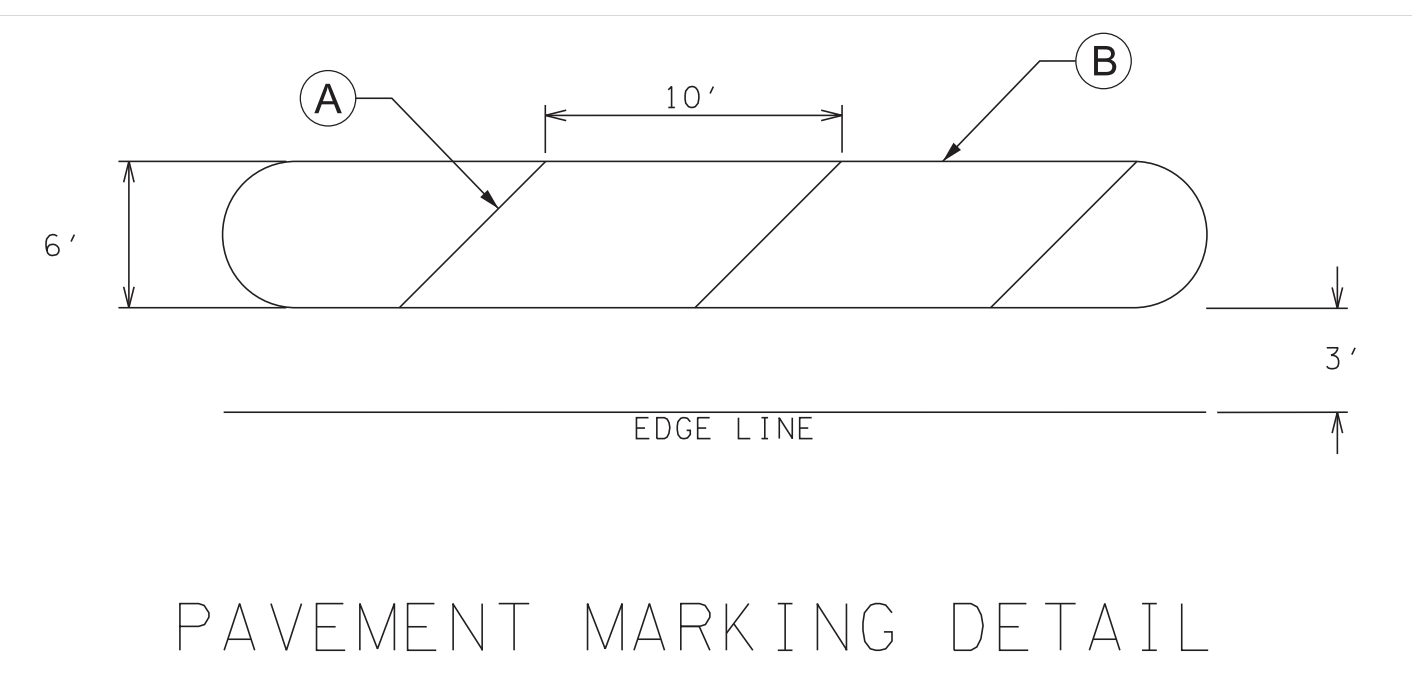
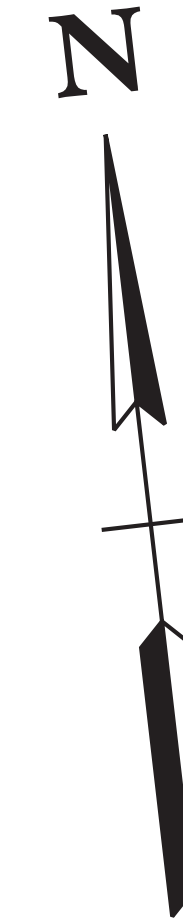
THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2024	STP/HSIP-152(25)	5



A 12" DIAGONAL TRANSVERSE CHANNELIZATION MARKINGS
[ITEM NO. 716-04.04]

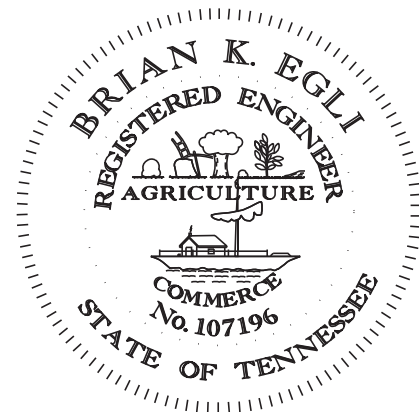
B 8" SINGLE SOLID WHITE LINE BOUNDARY
[ITEM NO. 716-02.04]

L.M. 5.759 LT.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
DETAILS

SCALE: N.T.S.



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Brian Egli
2023.12.04 11:02:37 -06'00'

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME **SHEET NO.**

SIGNATURE SHEET _____ STRUCTURE-SIGN 1
BRIDGE PLANS _____ B1 THRU B5

YEAR	PROJECT NO.	SHEET NO.
2024	17S152-M3-003	STRUCTURE-SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

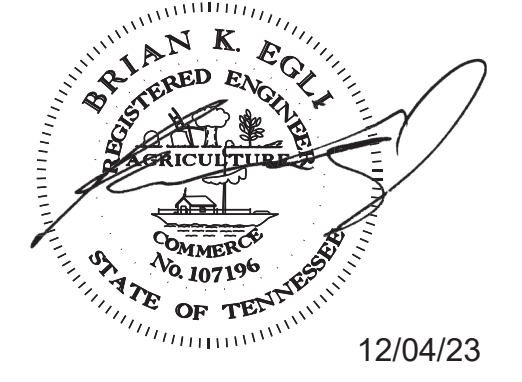
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PIN 132541.00

PROJECT NO.	YEAR	SHEET NO.	
17S152-M3-003	2024	B1	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

<u>INDEX OF DRAWINGS</u>	<u>DWG. NO.</u>	<u>LAST REV. DATE</u>
INDEX OF DRAWINGS-----	B1	
BRIDGE TABULATION AND ESTIMATED QUANTITIES-----	B2	
EXPANSION JOINT REPAIR NOTES-----	B3	
PLAN VIEW (17SR1520003)-----	B4	
PHASE CONSTRUCTION-----	B5	

<u>INDEX OF REFERENCE DRAWINGS</u>	<u>DWG. NO.</u>
LAYOUT OF BRIDGE-----	M-333-1
SUPERSTRUCTURE-----	M-333-4



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 INDEX OF DRAWINGS
 17-SR152-7.32
 OVER
 CYPRESS CREEK
 BR. NO. 17SR1520003
 CROCKETT COUNTY
 2024

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 9/23
 SUPERVISED BY K.MARTINKO DATE 9/23
 CHECKED BY _____ DATE _____

 EASY TIME *****
 10:51:53 AM

PIN 132541.00

PROJECT NO.	YEAR	SHEET NO.	
17S152-M3-003	2024	B2	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.44 EXPANSION JOINT REPAIR L.F.
17-SR152-7.32 OVER CYPRESS CREEK (17SR1520003)	M-333-1 M-333-4	EXPANSION JOINT REPAIR	58
TOTAL			58

ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	36
712-05.01	WARNING LIGHTS (TYPE A)	EACH	27
712-05.03	WARNING LIGHTS (TYPE C)	EACH	9
712-06	SIGNS (CONSTRUCTION)	S.F.	64
713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	2

DESCRIPTION	NO. REQ'D.	SIZE	SIGN I.D. NO.	QUANTITY	UNIT
ROAD WORK 1500 FT	2	48 X 48	W20-1	32	S.F.
BE PREPARED TO STOP	2	48 X 48	W3-4	32	S.F.
ITEM 712-06 SIGNS (CONSTRUCTION)				64	S.F.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

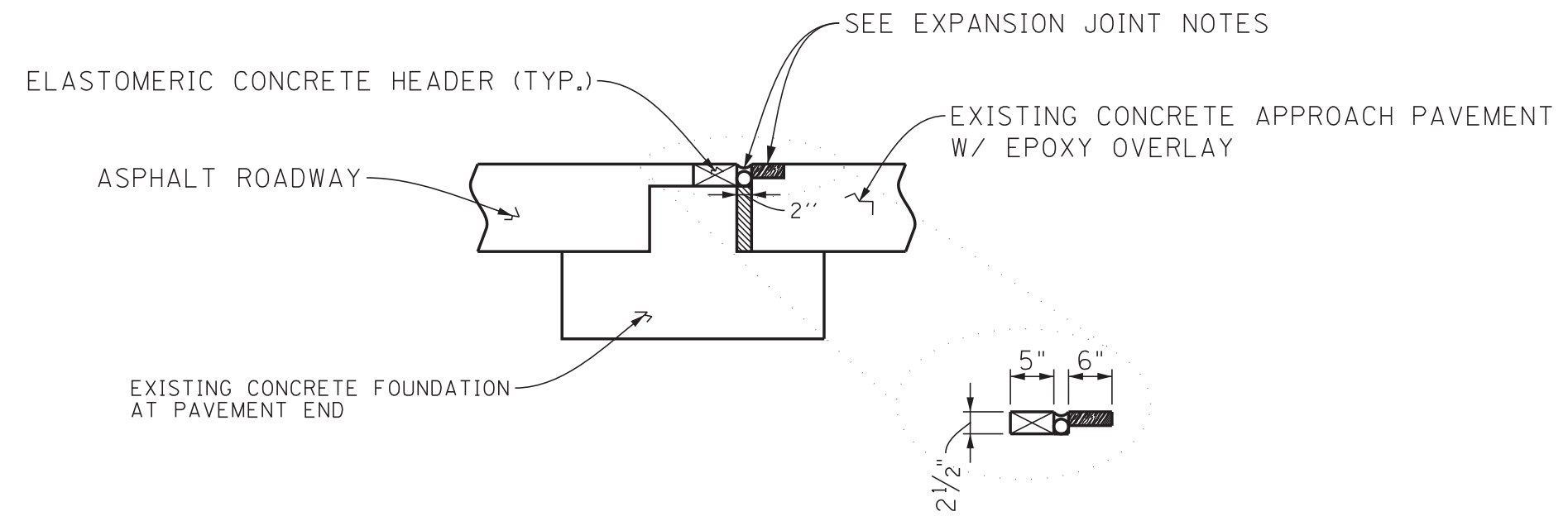
BRIDGE TABULATION AND
ESTIMATED QUANTITIES
17-SR152-7.32
OVER
CYPRESS CREEK
BR. NO. 17SR1520003
CROCKETT COUNTY
2024

DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 9/23
SUPERVISED BY K.MARTINKO DATE 9/23
CHECKED BY _____ DATE _____

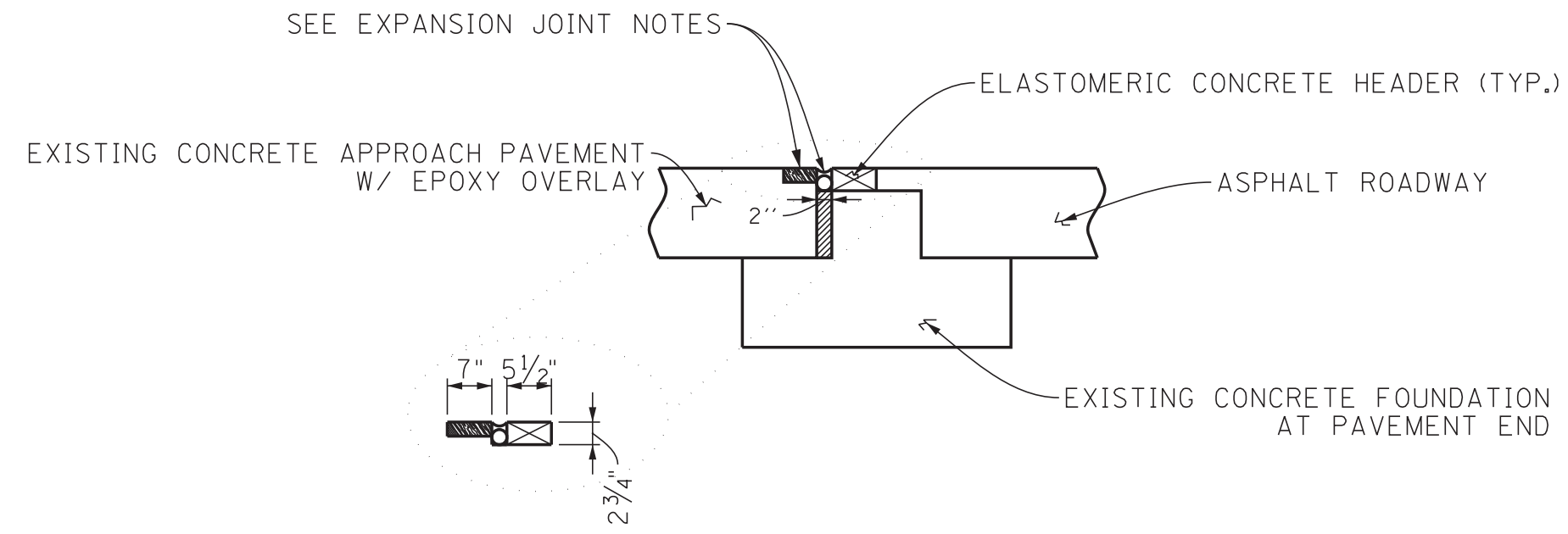
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PROJECT NO.	YEAR	SHEET NO.	
17S152-M3-003	2024	B3	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



EXPANSION JOINT REPAIR DETAIL
@ APPROACH END NO. 1



EXPANSION JOINT REPAIR DETAIL
@ APPROACH END NO. 2

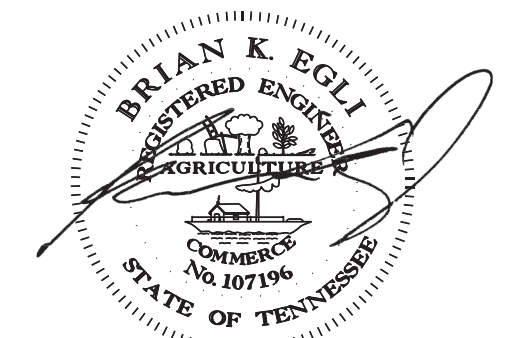
EXPANSION JOINT REPAIR NOTES

THE JOINT HEADER SYSTEM SHALL BE FROM OPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM OPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING, AND AN ELASTOMERIC CONCRETE FROM OPL 9.001. THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM OPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE OPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF 3/4 OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REQUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

PRIOR TO THE INSTALLATION OF THE NEW JOINT, THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT, DEBRIS, AND PRIOR CONSTRUCTION MATERIAL, ETC., THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE, SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION, TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

THE COST FOR REMOVING THE OLD JOINT SYSTEM, INSTALLING THE NEW JOINT SYSTEM, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT, IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.



12/04/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT REPAIR
NOTES
17-SR152-7.32
OVER
CYPRESS CREEK
BR. NO. 17SR1520003
CROCKETT COUNTY
2024

DESIGNED BY: Z. HAYNES DATE: 9/23
DRAWN BY: K. MARTINKO DATE: 9/23
SUPERVISED BY: K. MARTINKO DATE: 9/23
CHECKED BY: _____ DATE: _____

PIN 132541.00

PROJECT NO.	YEAR	SHEET NO.
17S152-M3-003	2024	B4

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

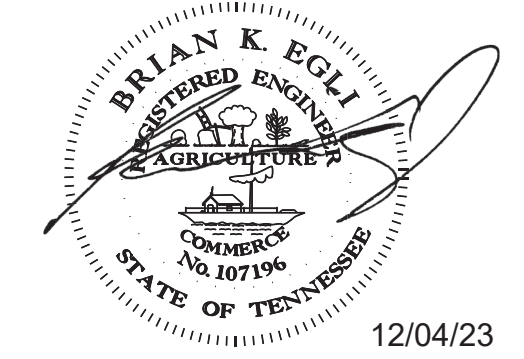


PLAN VIEW

BRIDGE ID. NO. 17SR1520003

 TIME: 10:53:18 AM
 DATE: 9/23/24

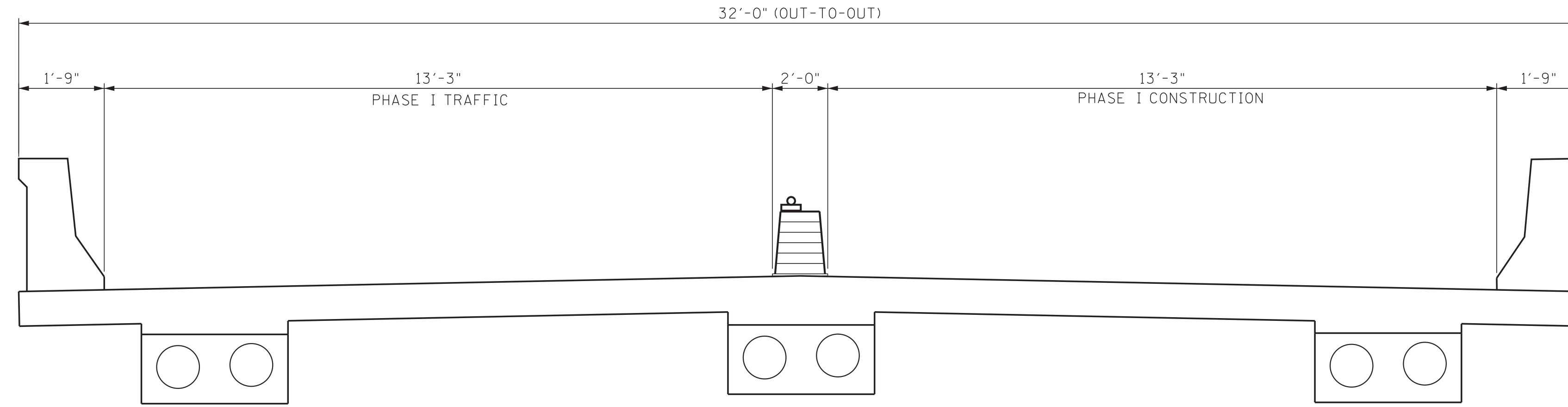
DESIGNED BY _____ DATE _____
 DRAWN BY Z. HAYNES DATE 9/23
 SUPERVISED BY K. MARTINKO DATE 9/23
 CHECKED BY _____ DATE _____



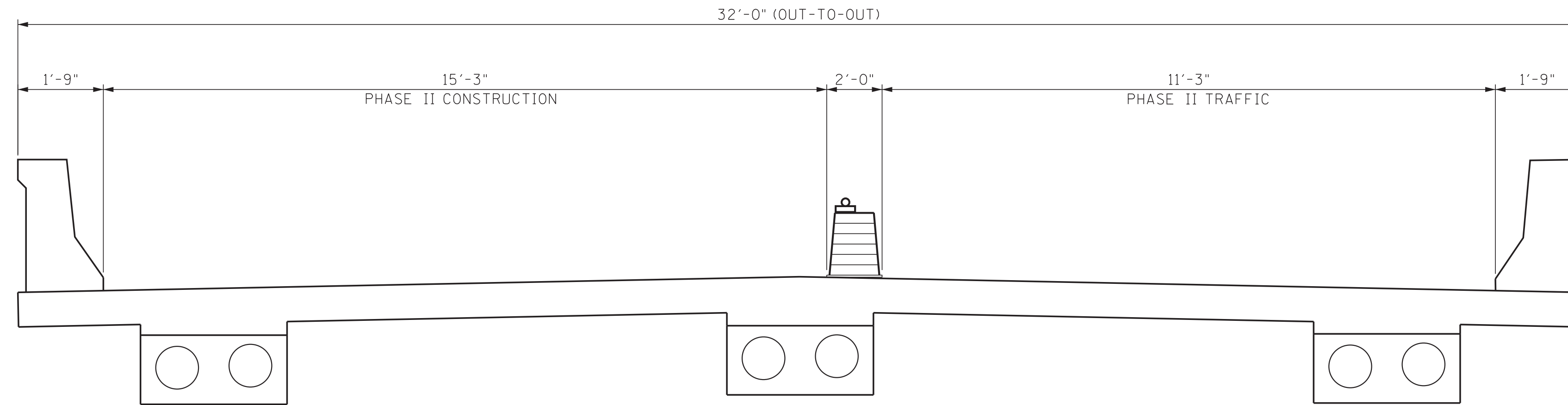
12/04/23
 STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 PLAN VIEW
 17-SR152-7.32
 OVER
 CYPRESS CREEK
 BR. NO. 17SR1520003
 CROCKETT COUNTY
 2024

PIN 132541.00

PROJECT NO.	YEAR	SHEET NO.	
17S152-M3-003	2024	B5	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



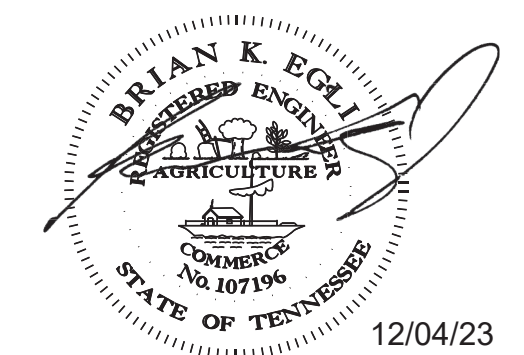
EASTBOUND BRIDGE
 (17-SR152-7.32)
 PHASE I CONSTRUCTION
 (LOOKING AHEAD ON SURVEY)



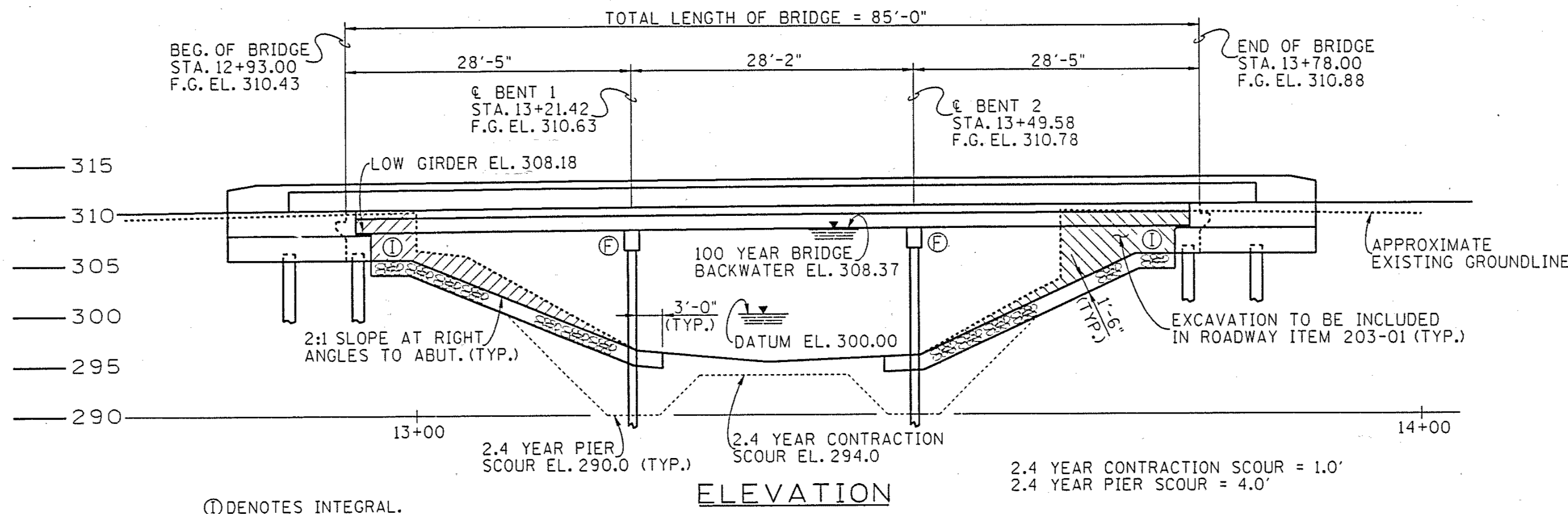
EASTBOUND BRIDGE
 (17-SR152-7.32)
 PHASE II CONSTRUCTION
 (LOOKING AHEAD ON SURVEY)

 10:54:06 AM

DESIGNED BY _____	DATE _____
DRAWN BY <u>Z. HAYNES</u>	DATE <u>9/23</u>
SUPERVISED BY <u>K. MARTINKO</u>	DATE <u>9/23</u>
CHECKED BY _____	DATE _____

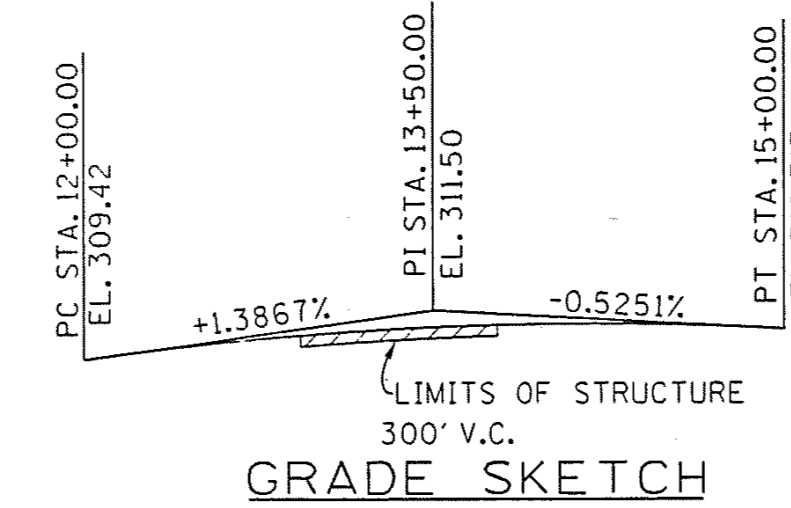


STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 PHASE CONSTRUCTION
 17-SR152-7.32
 OVER
 CYPRESS CREEK
 BR. NO. 17SR1520003
 CROCKETT COUNTY
 2024



HYDRAULIC DATA

THIS STRUCTURE AT LOG MILE 7.29
 DRAINAGE AREA = 38 MI.²
 TOTAL DESIGN DISCHARGE (2.4 YR.) = 3000 CFS
 DESIGN DISCHARGE (2.4 YR.) = 2395 CFS
 WATER AREA PROVIDED BELOW EL. 305.88 = 531 FT²
 2.4 YR. VELOCITY = 4.51 FPS
 2.4 YR. BRIDGE BACKWATER = .51 FT. @ EL. 306.39
 ROADWAY OVERTOPPING EL. = 306.40
 100 YR. DISCHARGE 7880 @ EL. 308.37
 STRUCTURE AT LOG MILE 7.38
 DESIGN DISCHARGE (2.4 YR.) = 283 CFS
 WATER AREA PROVIDED BELOW EL. 305.88 = 81 FT²
 2.4 YR. VELOCITY = 3.47 FPS
 STRUCTURE AT LOG MILE 7.18
 DESIGN DISCHARGE (2.4 YR.) = 319 CFS
 WATER AREA PROVIDED BELOW EL. 305.88 = 99 FT²
 2.4 YR. VELOCITY = 3.23



CONST. NO. 17008-3210-94

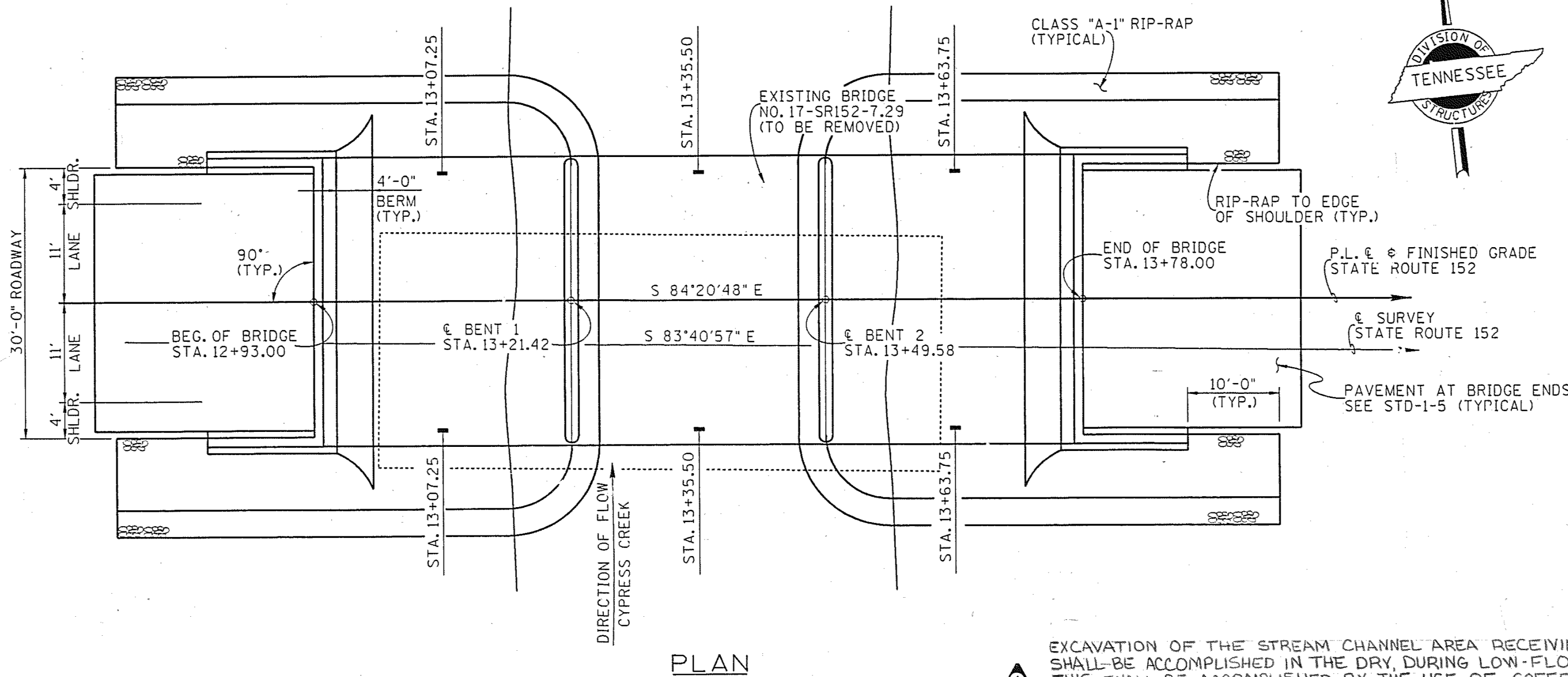
PROJECT NO.	YEAR	SHEET NO.
BR-STP-152(10)	1996	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	9-3-96	AEP	ADDED NOTE & REV. DATE
2	12-3-96	TCT	CHANGED DWG. NO. & REV. DATE

LIST OF DRAWINGS

	DWG. NO.	LATEST REV. DATE
LAYOUT OF BRIDGE	M-333-1	12-03-96
GENERAL NOTES AND ESTIMATED QUANTITIES	M-333-2	12-03-96
FOUNDATION DATA	M-333-3	
SUPERSTRUCTURE	M-333-4	
SUPERSTRUCTURE DETAILS	M-333-5	
PRESTRESSED BOX BEAM DETAILS	M-333-6	
ABUTMENT NO. 1 AND 2	M-333-7	
ABUTMENT NO. 1 AND 2 DETAILS	M-333-8	
BENT NO. 1 AND 2	M-333-9	
FINAL FOUNDATION DATA	M-333-10	12-03-96
BILL OF STEEL	M-333-11	



LIST OF STANDARD DRAWINGS

	DWG. NO.	LATEST REV. DATE
* BRIDGE RAILING CONCRETE PARAPET	STD-1-1	12-18-95
* SLIDER PLATES AND DECK DRAINS	STD-1-2	9-11-95
* PAVEMENT AT BRIDGE ENDS	STD-1-5	12-18-95
* STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-1	6-10-96
* STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STD-4-2	6-10-96
* STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-3	6-10-96
* STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STD-4-4	6-10-96
STD. PILE DETAILS	STD-5-1	10-25-93
STD. PILE DETAILS	STD-5-2	10-26-92
STANDARD SEISMIC DETAILS	STD-6-1	5-01-95
REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLAB	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS	STD-10-1	5-11-92
STANDARD DETAILS FOR PRESTRESSED BOX BEAMS	STD-14-3	

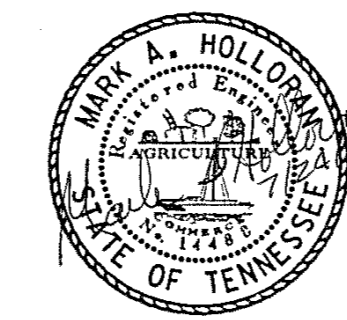
* DENOTES: THIS STANDARD TO BE PRINTED WITH THE PLANS.

LIST OF SPECIAL PROVISIONS

APPROVAL OF SHOP DRAWINGS	PROV. NO.	LATEST REV. DATE
-----	105A	3-6-95

2016 ADT = 1845
 30'-0" ROADWAY WITH STD-1-1 PARAPET
 DESIGN SPEED = 50 mph

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 LAYOUT OF BRIDGE
 STATE ROUTE 152
 OVER
 CYPRESS CREEK
 BR. I.D. NO. 17SR1520003
 STATION 13+35.50
 LOG MILE 7.29
 CROCKETT COUNTY
 1996

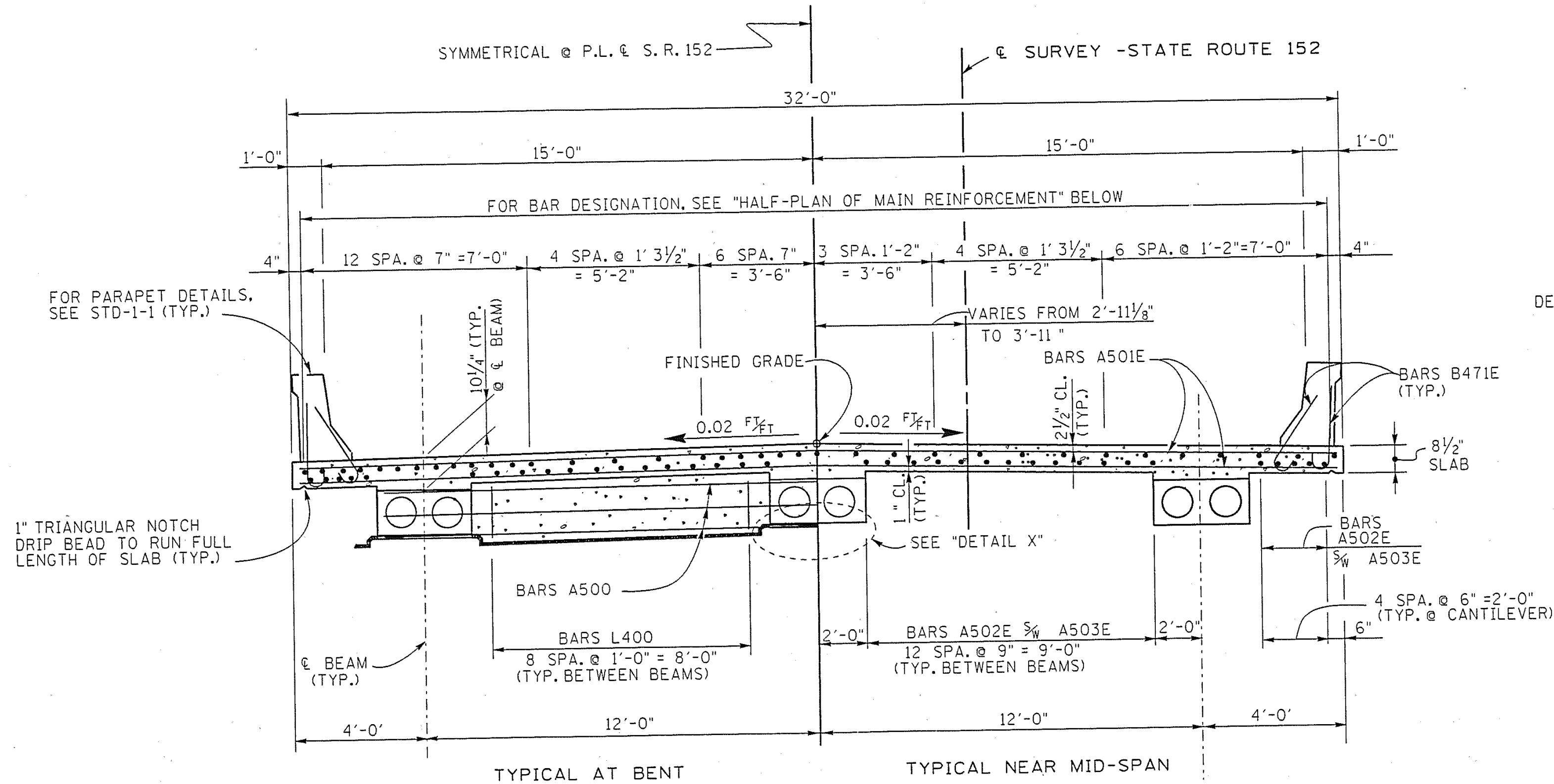


CORRECT *Edward P. Wasserman*
 ENGINEER OF STRUCTURES

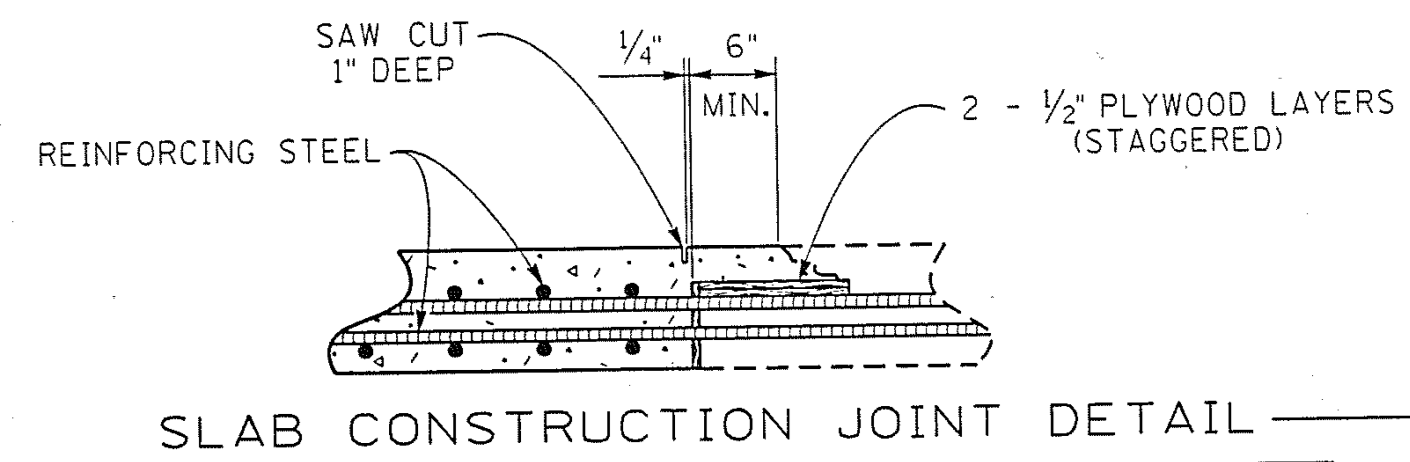
M-333-1

DESIGNED BY: (DL) TERI C. JOHNSON DATE 5-95/11-95
 DRAWN BY: M.R. MANNCHEN DATE 1-96
 SUPERVISED BY: RLH/RAP/JHW DATE 1-96
 CHECKED BY: TERI C. JOHNSON/AEP DATE 3-96/7-96

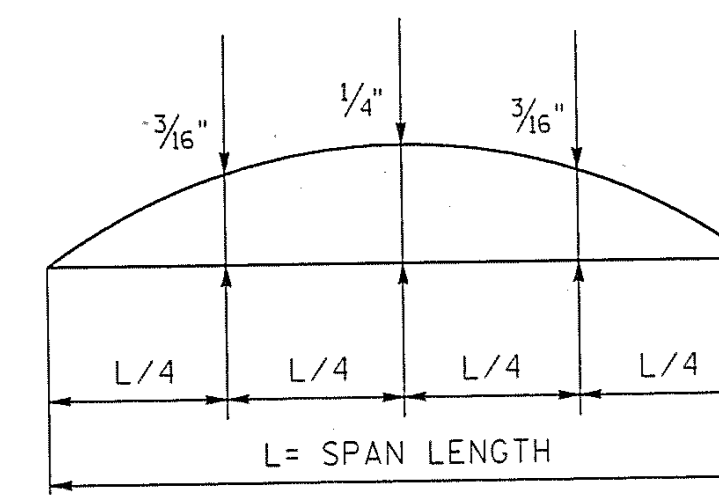
CONST. NO. 17008-3210-94			
PROJECT NO.	YEAR	SHEET NO.	
BR-STP-152(10)	1996		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	12-3-96	TCT	CHANGED DWG. NO.



TYPICAL CROSS SECTION
(LOOKING FORWARD ON SURVEY)

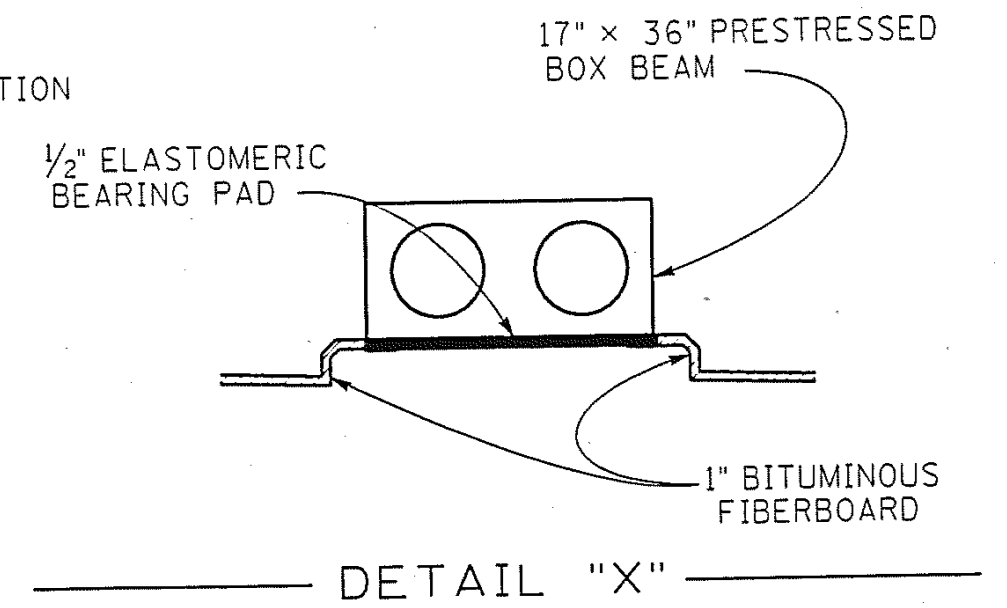


DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:
 1) NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT.
 2) THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FEET OF THE INTERIOR SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.
 ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE "SLAB CONSTRUCTION JOINT DETAIL" SHOWN ABOVE.

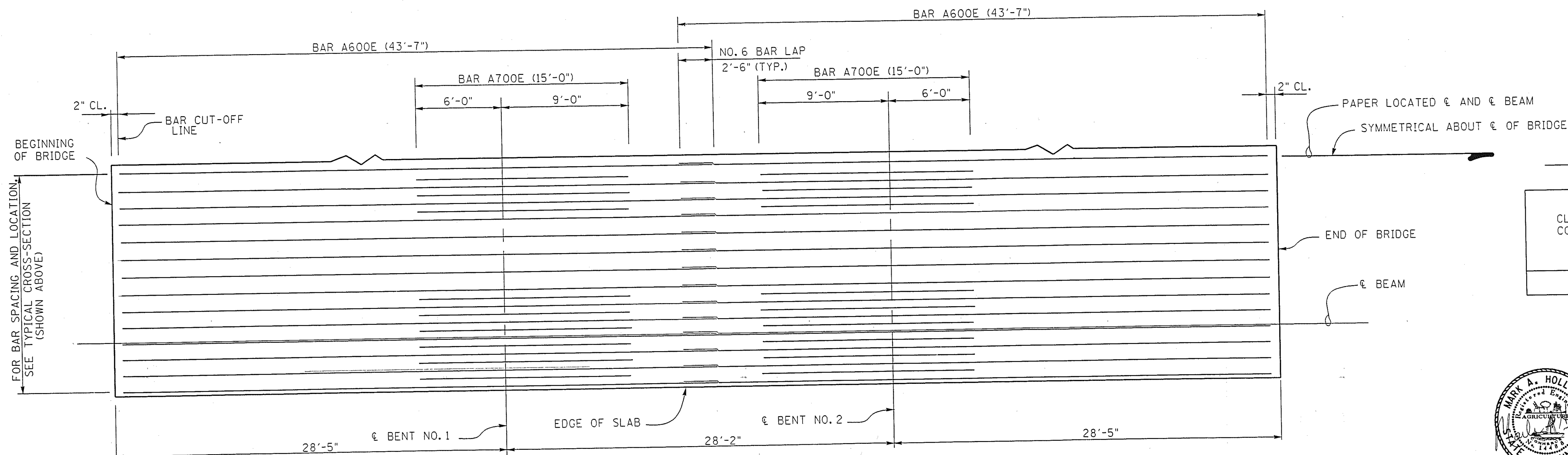


DEAD LOAD CORRECTION CURVE

NOTE: THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE. IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER THE PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.



DETAIL "X"

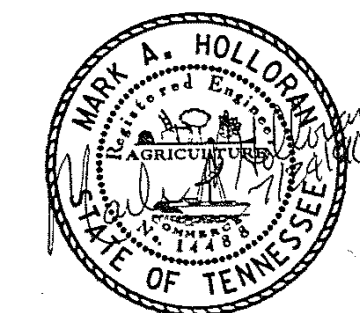


HALF-PLAN OF MAIN REINFORCEMENT

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR BRIDGE RAIL. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO SEE DRAWING NO. STD-1-1 AND M-328-26.
 NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

ESTIMATED QUANTITIES

CLASS "D" CONCRETE	EPOXY COATED STEEL BAR REINFORCEMENT	STEEL BAR REINFORCEMENT
C.Y.	LBS.	LBS.
79	19,562	516



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 SUPERSTRUCTURE
 STATE ROUTE 152
 OVER
 CYPRESS CREEK
 STATION 13+35.50
 LOG MILE 7.29
 CROCKETT COUNTY
 1996

CORRECT *Edward P. Wasserman*
 ENGINEER OF STRUCTURES

DESIGNED BY TERTI C. JOHNSON DATE 11-95/8-96
 DRAWN BY MORGAN R. MANNCHEN DATE 4-96
 SUPERVISED BY RLH/RAP DATE 4-96
 CHECKED BY TERTI C. JOHNSON/AEP DATE 4-96/7-96